

Date: June 27, 2014

RE: Duluth Airport Authority - Sky Harbor Apron Rehabilitation - Phase 2

ADDENDUM NO 2

TO: Prospective Bidders

This Addendum forms a part of the Contract Documents and modifies the original Bidding Documents dated June 12, 2014. Acknowledge receipt of this Addendum in the space provided on the Bid Form. Failure to do so may subject the Bidder to disqualification.

#### **INVITATION TO BID:**

No Changes

### **BID FORM:**

No Changes

### **TECHNICAL SPECIFICATIONS:**

### **MNDOT 2360 Bituminous Paving**

Add the Following:

The first bituminous lift shall be paved utilizing string line. Prior to the second lift of bituminous the area must be surveyed to verify a uniform 2" second lift. If the survey results indicate significant variations the second lift must be paved utilizing a string line.

The first pass shall be made using a taut stringline on both sides of the paver. Subsequent passes shall be made using a short ski or shoe on the previously placed lane and taut stringline on the other side. The stringline shall be mounted on sensor brackets every 25-feet on straight sections and laid out in cords along curves. Cord length will be dependent on the radius of the curve. The shorter radius curves shall have closer spacing of sensor brackets. The stringline shall extend onto the existing pavement, or the previous mat, at least 50-feet prior to pullout, so that the paver runs on automation.

Setting grade stakes and taut stringline includes establishing all of the conditions necessary for the stringline to adequately serve for grade referencing. The factors include but are not limited to:

- a. Setting grades away from the mat edge an additional distance to compensate for the extended distance of the wire from the stake.
- b. Setting the stringline at an established height, or relative height above finished grade.
- c. Stringline shall be anchored at both ends of the wire to reduce sagging.
- d. Additional anchors shall be used along curved sections to assist in keeping the stringline attached to the crossarm of the grade stake.
- e. Stringline shall be set as low as practical to help avoid disturbance by workers.



### **MNDOT 2360 Bituminous Paving**

The incentive pay for MnDOT 2360 will not apply. Incentives can be used to make up penalties, but the bid item will not be paid more than 100% of the total tons placed.

SPECIAL PROVISION S-7 (2504) WATER MAIN AND SERVICE LINE INSTALLATION.: ADD THE FOLLOWING: "THE CONTRACTOR SHALL APPLY FOR A CITY PLUMBING PERMIT AND INSTALL THE NEW WATER MAIN AND VALVES PER THE PLUMBING CODE AS WELL AS THE CURRENT STANDARD CONSTRUCTION SPECIFICATIONS. THE COST FOR THE PLUMBING PERMIT SHALL BE INCIDENTAL TO THE WATER MAIN.

#### **DRAWINGS**

Sheet C100 "Demolition Plan" See Attached. Sheet C700 "Pavement Marking Plan" See Attached Sheet C800 "Utility Plan" See Attached

#### Questions:

Q: Are there any schedule aircraft which must be accommodated?

A: Monday and Fridays mornings two twin engine Beechcraft float planes must be able to be towed to the seaplane ramp.

All else remains the same.

Sincerely,

Reynolds, Smith and Hills, Inc.

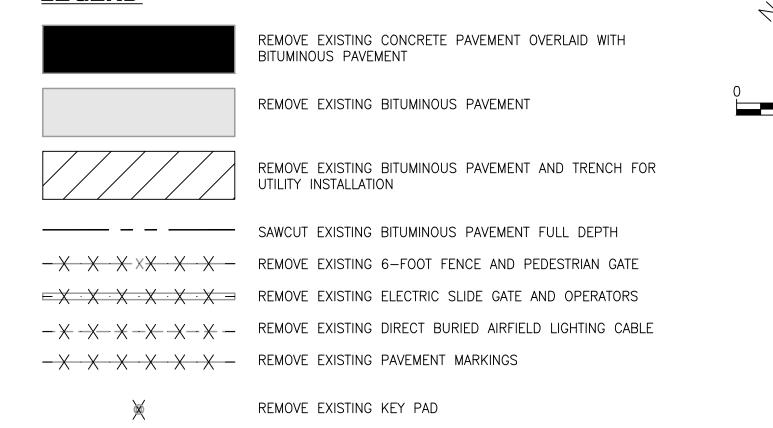
Darren K. Christopher, PE

**END OF ADDENDUM NO. 2** 

# **DEMOLITION NOTES**

- 1. CONTRACTOR SHALL REPAIR ANY AND ALL EXISTING PAVEMENT TO REMAIN THAT IS DAMAGED DURING CONSTRUCTION ACTIVITIES. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR REPAIRING EXISTING PAVEMENT TO REMAIN.
- 2. TRENCHING FOR UTILITY INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UTILITY INSTALLATION. FOLLOWING COMPLETION OF ACCEPTED UTILITY INSTALLATION, EACH PAVEMENT LAYER SHALL BE RECONSTRUCTED TO MATCH THE MATERIAL AND THICKNESS OF THE ADJACENT EXISTING PAVEMENT.
- 3. SEE SHEET C200 FOR COORDINATES OF PAVEMENT REMOVAL AND RECONSTRUCTION.

## **LEGEND**



REMOVE EXISTING AIRCRAFT TIE-DOWNS



Reynolds, Smith and Hills, Inc 4525 Airport Approach Road, Suite A Duluth, Minnesota 55811

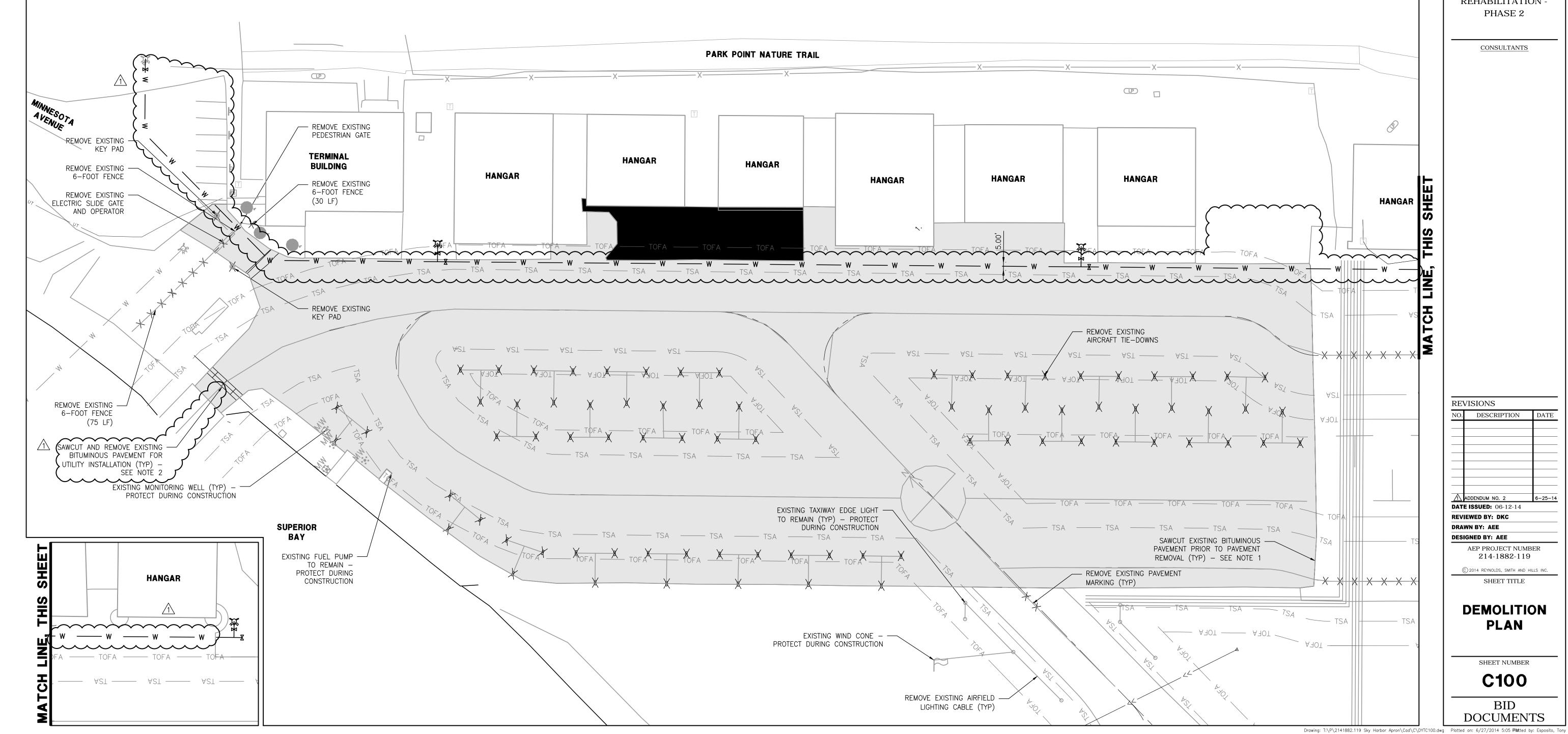
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**DULUTH AIRPORT AUTHORITY** 

SKY HARBOR AIRPORT APRON **REHABILITATION** -PHASE 2

CONSULTANTS



REVISIONS NO. DESCRIPTION ADDENDUM NO. 2 **DATE ISSUED:** 06-12-14 **REVIEWED BY: DKC** DRAWN BY: AEE DESIGNED BY: AEE AEP PROJECT NUMBER 214-1882-119 © 2014 REYNOLDS, SMITH AND HILLS INC. SHEET TITLE

**DEMOLITION** 

**PLAN** 

SHEET NUMBER

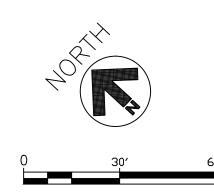
C100

BIDDOCUMENTS PAVEMENT MARKING NOTES 1. PAVEMENT MARKINGS SHALL MATCH EXISTING MARKINGS BEYOND THE EDGE OF PROPOSED PAVEMENT AS INDICATED IN THE PLAN. 

**LEGEND** 

NEW PAVEMENT MARKINGS

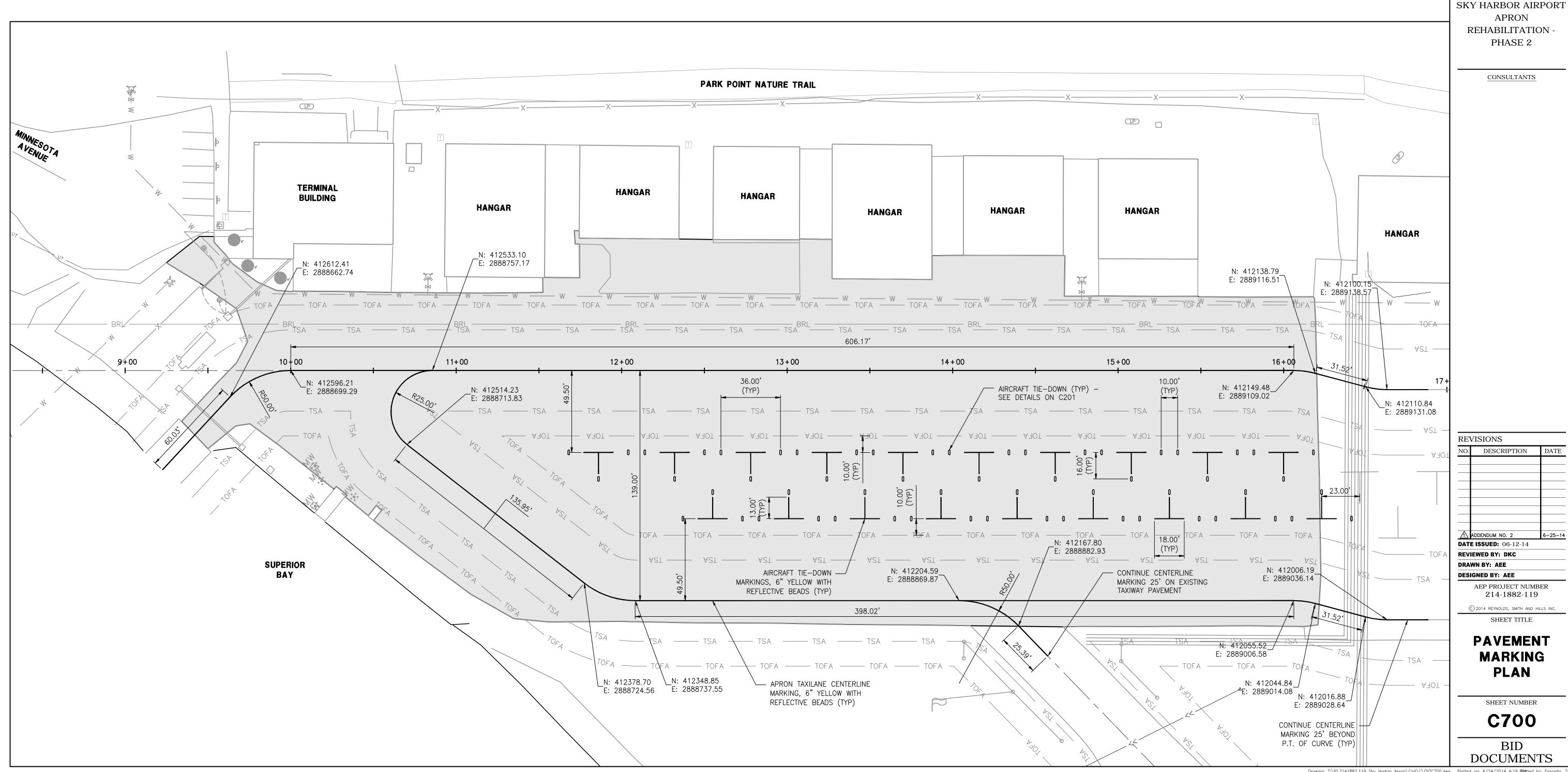
NEW AIRCRAFT TIE-DOWN



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# **UTILITY NOTES**

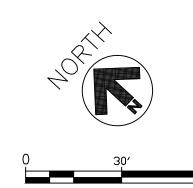
- 1. CONTRACTOR SHALL CONTINUE ELECTRICAL SERVICE TO MAIN CIRCUIT BREAKER INSIDE TERMINAL BUILDING.
- 2. REFER TO SHEET C805 FOR WATER SERVICE DETAILS.
- 3. ANY PAVEMENT DAMAGED FOR DIRECTIONAL DRILLING OF WATER MAIN SHALL BE REPLACED WITH LIKE MATERIALS (PCC PAVEMENT REPLACEMENT IN PCC PAVEMENT AND BITUMINOUS PAVEMENT REPLACEMENT IN BITUMINOUS PAVEMENT). NO SEPARATE PAYMENT SHALL BE MADE TO REPLACE DAMAGED PAVEMENT FROM DIRECTIONAL DRILLING.

## **LEGEND**

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--- W ---- W --- NEW WATER MAIN — — — — — — NEW DIRECT BURIED ELECTRICAL CABLE NEW 4-WAY, 3" DUCT NEW FIRE HYDRANT NEW WATER VALVE NEW ELECTRICAL MANHOLE OR HANDHOLE

NEW CARD READER





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